

INFORMATION REPORT INFORMATION REPORT
CENTRAL INTELLIGENCE AGENCY

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50X1-HUM

COUNTRY USSR (Krasnodar Kray)

REPORT

SUBJECT Restricted Zone near Makopse

DATE DISTR.

23 August 1960

NO. PAGES

1

REQUIREMENT

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DATE OF INFO.

PLACE & DATE ACQ.

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A five-page report on a restricted area located near Makopse /N 44-02, E 39-10/

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description and a sketch of the restricted area which was situated just north of the Makopse RR station along the railroad line between Sochi and Tuapse. The site was rail-served, enclosed by a barbed-wire fence 2.5 meters high, and covered an area approximately one kilometer square. Also contained in the report is a description of the Krasnodar/Novorossiysk highway construction begun in 1949 and a location sketch of a submarine base in Gelendzhik [possibly N 44-34, E 38-03].

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COUNTRY: USSR (Krasnodarskiy
Kray, RSFSR)

SUBJECT:

Restricted Zone
located near Makapse

REPORT

DATE O

DATE A

PLACE ACQUIRED

DATE OF REPORT: 26 July 1960

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RESTRICTED ZONE LOCATED NEAR MAKAPSE

L. A restricted area was located some 500 meters north of the Makapse railroad station (N 44-02, E 39-10), Krasnodarskiy Kray, RSFSR; a village by the same name was approximately one kilometer northeast of this restricted area. (For location of this area, refer to sketch No. 1, overlay of Soviet map of Krasnodarskiy Kray, on page 4.) This zone was near a mountain and occupied an approximately one-kilometer long tract parallel to the railroad line which led northward from the Makapse railroad station to Tuapse; it was about one kilometer in depth (i.e., from west to east) and its northern boundary was formed by a small river (name not known) which emptied into the Black Sea. The area was surrounded by a barbed-wire fence, about two meters high, topped by another half meter of barbed-wire which angled inward. There were

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two entrances in the portion of the wire fence which faced the sea; one was for personnel and the other for a railroad spur which connected with the railroad leading to Tuapse. Within the restricted area, the railroad spur diverged into two spurs, one longer than the other: the shorter spur entered a tunnel (see sketch No. 2. on page 5).

2. Within the zone one could see that the mountain had been cut vertically and that a tunnel, approximately six meters wide and seven meters high, had been constructed in the mountaininside: as stated above, a railroad spur entered the tunnel.

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Stationed along the longer railroad spur were two separate groups of railroad cars, irregular in shape; they were completely covered with tarpaulins down to the ground

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each group of cars was from 38 to 40 meters long (the approximate length of three railroad cars) and some four meters high. Judging by the shape of the covering tarpaulins, the center portion of each group resembled a flat car with artillery; the anterior and posterior portions resembled ordinary railroad cars with high sides. Guards were on duty beside each group of cars.

3. In the area along the river bank could be seen a row of eight identical wooden barracks, about 30 meters long and eight meters high; the first one, which was visible from the railroad line, had 14 windows, approximately 1.20 meters wide and 1.40 meters high. Between the barracks and the river were seven or eight gray quadrangular army tents, about ten meters long and six meters wide. the barracks and tents served as billets for naval personnel who were garrisoned in the area.

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4. On the barbed wire fence which surrounded the area was a sign which read "Prohibited Zone" (Zapretnaya Zona); the words were inscribed on a white background in letters 60 x 40 centimeters in size. At the rear of the area (i.e., the portion nearest Makapse) there was an athletic field with parallel bars, a basketball court, and other sports equipment.

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5. The naval forces garrisoned in the area wore navy blue uniforms and round white navy caps. All the sentries were armed with rifles.
6. There were no buildings in the Makapse railroad station; two railroad cars without wheels, situated by the sea, housed the railroad offices.
7. Set forth below is the legend for sketch No. 1, overlay of 50X1-HUM
[redacted] Krasnodarskiy Kray [redacted]

(1) Krasnodar-Neverossiysk highway: Construction of this highway was begun in 1949 [redacted] 50X1-HUM

[redacted] The shoulders were between two and 2.5 meters wide and the total width of the road, including the shoulders, varied between 16 and 17 meters. The total thickness of the roadbed was some 60 centimeters. It had three layers of asphalt: the first layer, from 10 to 12 centimeters thick, was made with coarse gravel; the second, some six centimeters thick, was made with fine gravel, and the third, between four and five centimeters in thickness, was made with very fine gravel and a great quantity of sand. [redacted] 50X1-HUM

[redacted] in the month of November alone, six or seven kilometers of roadway had been constructed. All the laborers [redacted] were convicts and various [prison] camps had been set up along the highway. This highway construction project was of military interest and, on several occasions [redacted] 50X1-HUM

[redacted] military officers visited the construction site; they were especially interested in bypassing cities and eliminating curves on the road. The highest ranking [redacted] of the visiting officers was an MVD colonel. 50X1-HUM

(2) Submarine base in Gelendzhik: It was said that a submarine base, with submarine shelters constructed above the rocks, was located in the bay [Gelendzhikskaya bukhta?] 50X1-HUM

(3) Restricted zone near Makapse.

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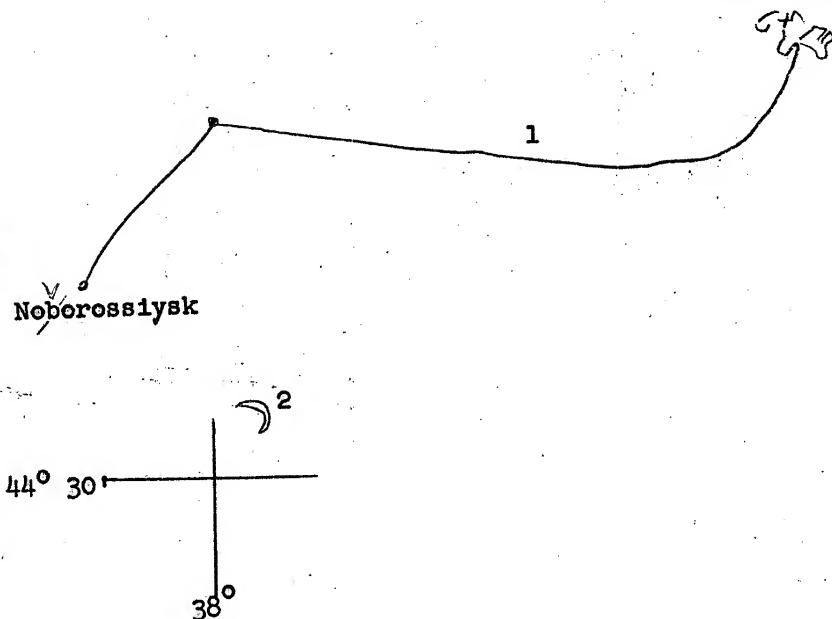
Sketch No. 1

Over
Kray

of Krasnodarskiy

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Krasnodar



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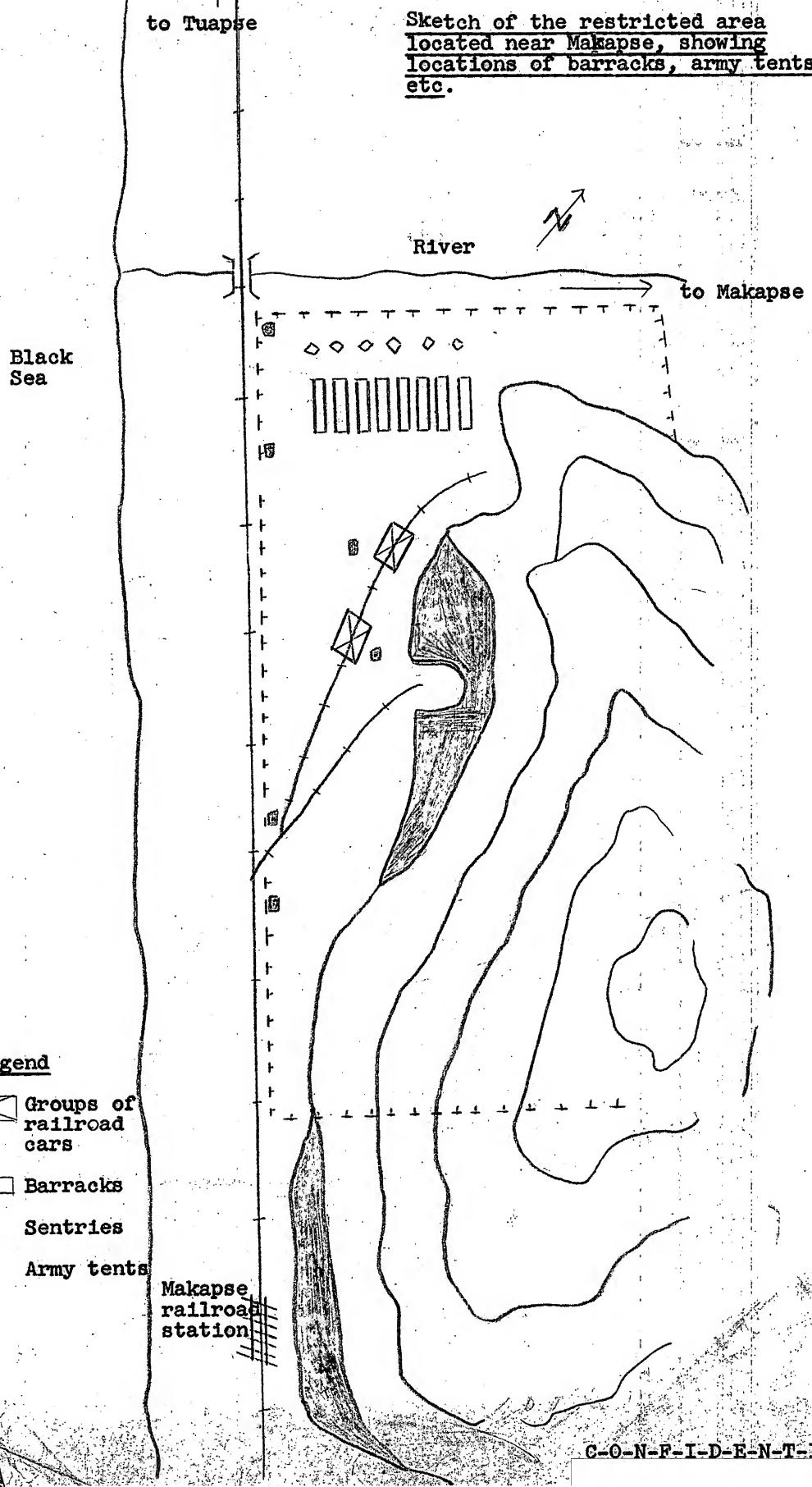
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Sketch No. 2

Sketch of the restricted area located near Makapse, showing locations of barracks, army tents, etc.



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